

Chapter 18.620
TIGARD TRIANGLE PLAN DISTRICT

Sections:

18.620.010	Purpose and Applicability
18.620.015	Where These Regulations Apply
18.620.020	Street Connectivity
18.620.030	Site Design Standards
18.620.040	Building Design Standards
18.620.050	Signs
18.620.060	Entry Portals
18.620.070	Landscaping and Screening
18.620.080	Street and Accessway Standards
18.620.090	Design Evaluation

18.620.010 Purpose and Applicability

- A. Design principles. Design standards for public street improvements and for new development and renovation projects have been prepared for the Tigard Triangle Plan District. These design standards address several important guiding principles adopted for the Tigard Triangle Plan District, including creating a high-quality mixed use employment area, providing a convenient pedestrian and bikeway system within the Triangle, and utilizing streetscape to create a high quality image for the area.
- B. Development conformance. All new developments, including remodeling and renovation projects resulting in uses other than single family residential use, are expected to contribute to the character and quality of the area. In addition to meeting the design standards described in this chapter and other development standards required by the community development and building codes, such developments will be required to:
1. Dedicate and improve public streets, to the extent that such dedication and improvement is directly related and roughly proportional to an impact of the development;
 2. Connect to public facilities such as sanitary sewer, water and storm drainage;
 3. Participate in funding future transportation and other public improvement projects in the Tigard Triangle Plan District, provided that the requirement to participate is directly related and roughly proportional to an impact of the development.
- C. Conflicting standards. The following design standards apply to all development located within the Tigard Triangle Plan District within both the C-G and the MUE zones. If a standard found in this section conflicts with another standard in the development code, standards in this section shall govern. (Ord. 13-04 §1; Ord. 99-22)

18.620.015 Where These Regulations Apply

The regulations of this chapter apply to the Tigard Triangle Plan District. The boundaries of this plan district are shown on Map 18.620.A at the end of this chapter and on the official zoning map. (Ord. 13-04 §1)

18.620.020 Street Connectivity

Demonstration of standards. All development must demonstrate how one of the following standard options will be met. Variance of these standards may be approved per the requirements of Section 18.370.010 where topography, barriers such as railroads or freeways, or environmental constraints such as major streams and rivers prevent street extensions and connections.

A. Design option.

1. Local street spacing shall provide public street connections at intervals of no more than 660 feet.
2. Bike and pedestrian connections on public easements or right-of-way shall be provided at intervals of no more than 330 feet.

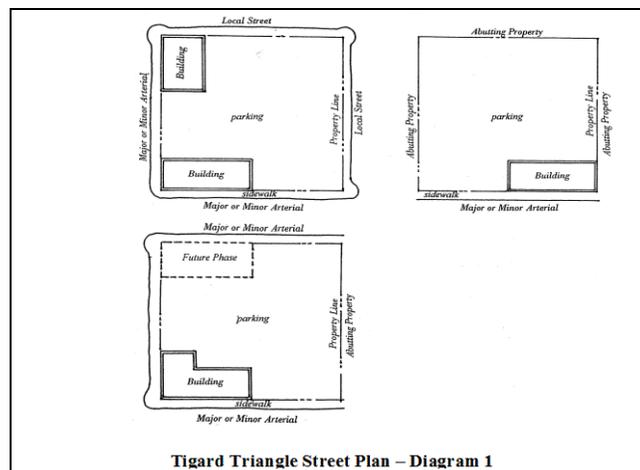
B. Performance option.

1. Local street spacing shall occur at intervals of no less than eight street intersections per mile.
2. The shortest vehicle trip over public streets from a local origin to a collector or greater facility is no more than twice the straight-line distance.
3. The shortest pedestrian trip on public right-of-way from a local origin to a collector or greater facility is no more than 1-1/2 the straight-line distance.

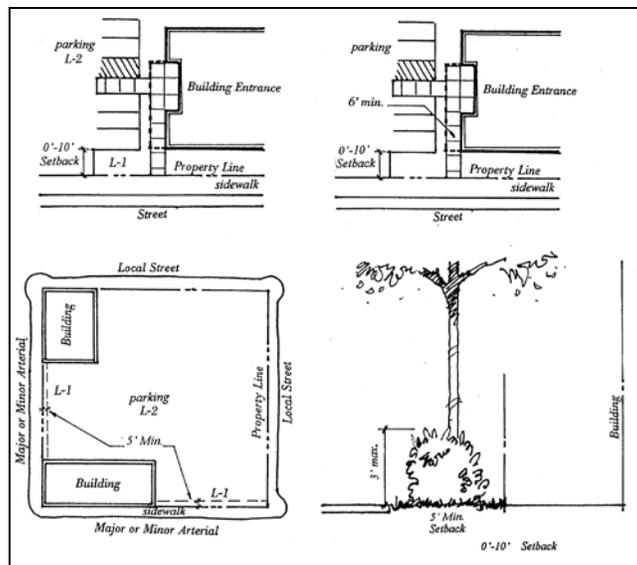
18.620.030 Site Design Standards

Compliance. All development must meet the following site design standards. If a parcel is one acre or larger a phased development plan must be approved demonstrating how these standards for the overall parcel can be met. Variance to these standards may be granted if the criteria found in Section 18.370.010.C.2, governing criteria for granting a variance, is satisfied.

- A. Building placement on major and minor arterials. Buildings shall occupy a minimum of 50% of all street frontages along major and minor arterial streets. Buildings shall be located at public street intersections on major and minor arterial streets. See Diagram 1 for some examples of how this standard may be met.



- B. Building setback. The minimum building setback from public street rights-of-way or dedicated wetlands/buffers and other environmental features shall be zero feet; the maximum building setback shall be 10 feet.
- C. Front yard setback design. Landscaping, an arcade, or a hard-surfaced expansion of the pedestrian path must be provided between a structure and a public street or accessway. If a building abuts more than one street, the required improvements shall be provided on all streets. Landscaping shall be developed to the applicable standard in paragraph 5 of this subsection A. Hard-surfaced areas shall be constructed with scored concrete or modular paving materials. Benches and other street furnishings are encouraged. These areas shall contribute to the minimum landscaping requirement per 18.520.040.B and Table 18.520.2.
- D. Walkway connection to building entrances. A walkway connection is required between a building's entrance and a public street or accessway. This walkway must be at least six feet wide and be paved with scored concrete or modular paving materials. Building entrances at a corner near a public street intersection are encouraged. These areas shall contribute to the minimum landscaping requirement per 18.520.040.B and Table 18.520.2.
- E. Parking location and landscape design. Parking for buildings or phases adjacent to public street rights-of-way must be located to the side or rear of newly constructed buildings. If located on the side, parking is limited to 50% of the street frontage and must be behind a landscaped area constructed to an L-1 landscape standard. The minimum depth of the L-1 landscaped area is eight feet or is equal to the building setback, whichever is greater. Interior side and rear yards shall be landscaped to an L-2 landscape standard, except where a side yard abuts a public street where it shall be landscaped to an L-1 landscape standard. See Diagram 2 below.



(Ord. 12-09 §1)

18.620.040 Building Design Standards

- A. Nonresidential buildings. All nonresidential buildings shall comply with the following design standards. Variance to these standards may be granted if the criteria found in 18.370.010.C.2, criteria for granting a variance, is satisfied.

1. Ground floor windows. All street-facing elevations within the building setback (zero to 10 feet) along public streets shall include a minimum of 50% of the ground floor wall area with windows, display areas or doorway openings. The ground floor wall area shall be measured from three feet above grade to nine feet above grade the entire width of the street-facing elevation. The ground floor window requirement shall be met within the ground floor wall area and for glass doorway openings to ground level. Up to 50% of the ground floor window requirement may be met on an adjoining elevation as long as all of the requirement is located at a building corner.
2. Building façades. Façades that face a public street shall extend no more than 50 feet without providing at least one of the following features: (a) a variation in building materials; (b) a building off-set of at least one foot; (c) a wall area that is entirely separated from other wall areas by a projection, such as an arcade; or (d) by another design features that reflect the building's structural system. No building façade shall extend for more than 300 feet without a pedestrian connection between or through the building.
3. Weather protection. Weather protection for pedestrians, such as awnings, canopies, and arcades, shall be provided at building entrances. Weather protection is encouraged along building frontages abutting a public sidewalk or a hard-surfaced expansion of a sidewalk, and along building frontages between a building entrance and a public street or accessway. Awnings and canopies shall not be back lit.
4. Building materials. Plain concrete block, plain concrete, corrugated metal, plywood, sheet press board or vinyl siding may not be used as exterior finish materials. Foundation material may be plain concrete or plain concrete block where the foundation material is not revealed for more than two feet.
5. Roofs and roof lines. Except in the case of a building entrance feature, roofs shall be designed as an extension of the primary materials used for the building and should respect the building's structural system and architectural style. False fronts and false roofs are not permitted.
6. Roof-mounted equipment. All roof-mounted equipment must be screened from view from adjacent public streets. Satellite dishes and other communication equipment must be set back or positioned on a roof so that exposure from adjacent public streets is minimized. Solar heating panels are exempt from this standard.

18.620.050 Signs

- A. Sign standards. In addition to the requirements of Chapter 18.780 of the development code the following standards shall be met:
 1. Zoning district regulations. Residential only developments within the C-G and MUE zones shall meet the sign requirements for the R-25 zone in 18.780.130.B; nonresidential developments within the C-G zone shall meet the sign requirements for the commercial zones in 18.780.130.C; and nonresidential development within the MUE zone shall meet the sign requirements of the C-P zone in 18.780.130.D.
 2. Sign area limits. The maximum sign area limits found in Section 18.780.130 shall not be exceeded. No area limit increases will be permitted within the Tigard Triangle Plan District.
 3. Height limits. The maximum height limit for all signs except wall signs shall be 10 feet. Wall signs shall not extend above the roof line of the wall on which the sign is located. No height increases will be permitted within the Tigard Triangle Plan District.

4. Sign location. Freestanding signs within the Tigard Triangle Plan District shall not be permitted within required L-1 landscape areas.

18.620.060 Entry Portals

Required locations. Entry portals shall be required at the primary access points into the Tigard Triangle Plan District.

- A. Location. Entry portals shall be located at the intersections of 99W and Dartmouth; 99W and 72nd; I-5 and Dartmouth; Highway 217 and 72nd; and at the Highway 217 overcrossing and Dartmouth.
- B. Design. The overall design of entry portals shall relate in scale and detail to both the automobile and the pedestrian. A triangle motif and at least two trees according to the L-2 standard shall be incorporated into the design of entry portals. (Ord. 12-09 §1)

18.620.070 Landscaping and Screening

Applicable levels. Two levels of landscaping and screening standards are applicable to the Tigard Triangle Plan District. The locations where the landscaping or screening is required and the depth of the landscaping or screening are defined in other subsections of this section. These standards are minimum requirements. Higher standards may be substituted as long as all height limitations are met.

- A. L-1 parking lot screen. The L-1 standard applies to setbacks on public streets. The L-1 standard is in addition to other standards in other chapters of this title. The setback shall be a minimum of eight feet between the parking lot and public street. L-1 trees shall be considered parking lot trees and spaced between 30 and 40 feet on center within the setback. All L-1 trees shall be a minimum of 3-1/2 inch caliper at the time of planting. Shrubs shall be of a variety that will provide a three-foot high screen and a 90% opacity within one year. Groundcover plants must fully cover the remainder of landscape area within two years.
- B. L-2 general landscaping. The L-2 standard applies to all other trees and shrubs required by this chapter and Chapter 18.745 (except those required for L-1 parking lot screen). For trees and shrubs required by Chapter 18.745, the L-2 standard is an additional standard. L-2 trees that are also street trees, median trees, and trees required to frame entry portals shall be selected in conformance with Table 18.620.1 of this section. If conformance with Table 18.620.1 is precluded by physical constraints caused by public utilities or required public improvements, the director may approve alternative selections. All L-2 trees shall be a minimum of 2-1/2-inch caliper at the time of planting. Shrubs shall be of a size and quality to achieve the required landscaping or screening effect within two years.

**Table 18.620.1
L-2 Tree Standards**

General Tree Type¹	Location	Specific Tree Type²
Street Tree	72nd Avenue	Large Stature Street Trees
Street Tree	Dartmouth Street	Large Stature Street Trees
Street Tree	68th Avenue	Columnar Trees
Street Tree	Atlanta Street	Columnar Trees
Street Tree	Hampton Street	Columnar Trees
Street Tree	66th Avenue	Large Stature Street Trees
Street Tree	Backage Road	Even Mix of Large, Medium, and Small Stature Street Trees
Street Tree	All other local streets	Medium Stature Street Trees
Median Tree	72nd Avenue	Large Stature Street Trees
Median Tree	68th Avenue	Large Stature Street Trees
Median Tree	Atlanta Street	Large Stature Street Trees
Median Tree	Hampton Street	Large Stature Street Trees
Portal Tree	All required portals	Columnar Trees

¹ When these tree types are required by Chapter 18.620, 18.745, or 18.790, the L-2 tree standards apply.

² Additional specifications for species, planting, and spacing are in the Urban Forestry Manual.

(Ord. 12-09 §1)

18.620.080 Street and Accessway Standards

Tables and diagrams. The following tables and diagrams show street and pedestrian accessway standards for the Tigard Triangle Plan District. Landscape and street design details are also included in this section.

**Table 18.620.2
Street Classification and Function Table: Streets**

Street	Policy Classification	Street Function	Land Use/ Design Priority	Right-of-Way
72nd Street	Major Arterial Transit Access Street Pedestrian-Transit Street Bikeway	Provide access to Triangle destinations	Mixed use employment	92 feet
Hwy 99 to Hwy 217		Distribute traffic within the Triangle Plan District	Limited access to off-street parking	66 feet curb-to-curb
		Provide connections between districts	Enhanced pedestrian environment	
		Distribute traffic from regional arterials and major collectors to local service streets	Boulevard design with two-way traffic	
		Local transit service	Transit-oriented street features	
		Bicycle mobility	Bike lanes	
			Continuity of alignment and design throughout the Triangle Plan District	

Table 18.620.2 (cont'd)

Street	Policy Classification	Street Function	Land Use/ Design Priority	Right-of-Way
Dartmouth Street Hwy 99 to 68th Ave.	Major Arterial Transit Access Street Pedestrian-Transit Street Bikeway	Provide access to Triangle Plan District destinations Distribute traffic within the Triangle Plan District Provide connections between districts Distribute traffic from regional arterials and major collectors to local service streets Local transit service Bicycle mobility	Mixed use employment with neighborhood and regional retail Limited access to off-street parking Enhanced pedestrian environment	92 feet - 72nd 94 feet - west of 72nd 72 feet - east of 72nd
68th Avenue Atlanta to Hampton	Minor Arterial Pedestrian-Transit Street	Provide access to local services Distribute local traffic Bicycle lanes	Mixed use employment Enhanced pedestrian environment Bicycle access	70 feet 46 feet curb-to-curb
217 Over-Crossing West of Dartmouth	Minor Arterial Transit Access Street Pedestrian-Transit Street	Provide access to Triangle Plan District destinations Distribute traffic within the Triangle Plan District Bicycle lanes Provide connections between districts Distribute traffic from arterials and collector streets to local service streets Local transit service	Mixed use employment Enhanced pedestrian environment Transit-oriented street features Bicycle access	70 feet 46 feet curb-to-curb
Hampton St. 68th to 72nd	Minor Arterial Transit Access Street Pedestrian-Transit Street	Provide access to Triangle Plan District destinations Distribute traffic within the Triangle Plan District Bicycle lanes Provide connections between districts Distribute traffic from arterials and collector streets to local service streets Local transit service	Mixed use employment Enhanced pedestrian environment Transit-oriented street features Bicycle access	70 feet 46 feet curb-to-curb

Table 18.620.2 (cont'd)				
Street	Policy Classification	Street Function	Land Use/ Design Priority	Right-of-Way
Backage Road	Local Collector	Provide access to local services	Mixed use commercial and retail along 99W	60 feet
		Distribute local traffic	Access to off-street parking	36 feet curb-to-curb
		Parking access street	Enhanced pedestrian environment	
East-West Streets	Local Service Street	Provide access to local services	Mixed use commercial and retail along 99W	60 feet
		Distribute local traffic	Access to off-street parking	34 feet curb-to-curb
		Parking access street	Enhanced pedestrian environment	
North-South Streets	Local Service Street	Provide access to local services	Mixed use commercial and retail along 99W	60 feet
		Distribute local traffic	Access to off-street parking	34 feet curb-to-curb
		Parking access street	Enhanced pedestrian environment	

Accessways

Class I

Function: Auto and parking (on at least one side)
 Access to parking (optional)
 Bicycles (in roadway)
 Pedestrian improvements

Right-of-way 50' - 60'

Ownership: Public dedication

Application: East-west, north-south streets to parking access

Class II

Function: Pedestrian and bicycle

Right-of-way 40' *

Ownership: Public or private with public access easement

Application: East-west, north-south accessways
 Fire access *
 Critical pedestrian access routes.

* If emergency vehicle access is provided by alternative locations, a lesser dimension to a minimum of 30 feet in width may be allowed with approval of the city engineer.

Class III

Function:	Pedestrian
Right-of-way	10' - 25' case-by-case
Ownership:	Private with public access easement
Application:	East-west, north-south accessways Connections to meet accessibility standards

Class IV

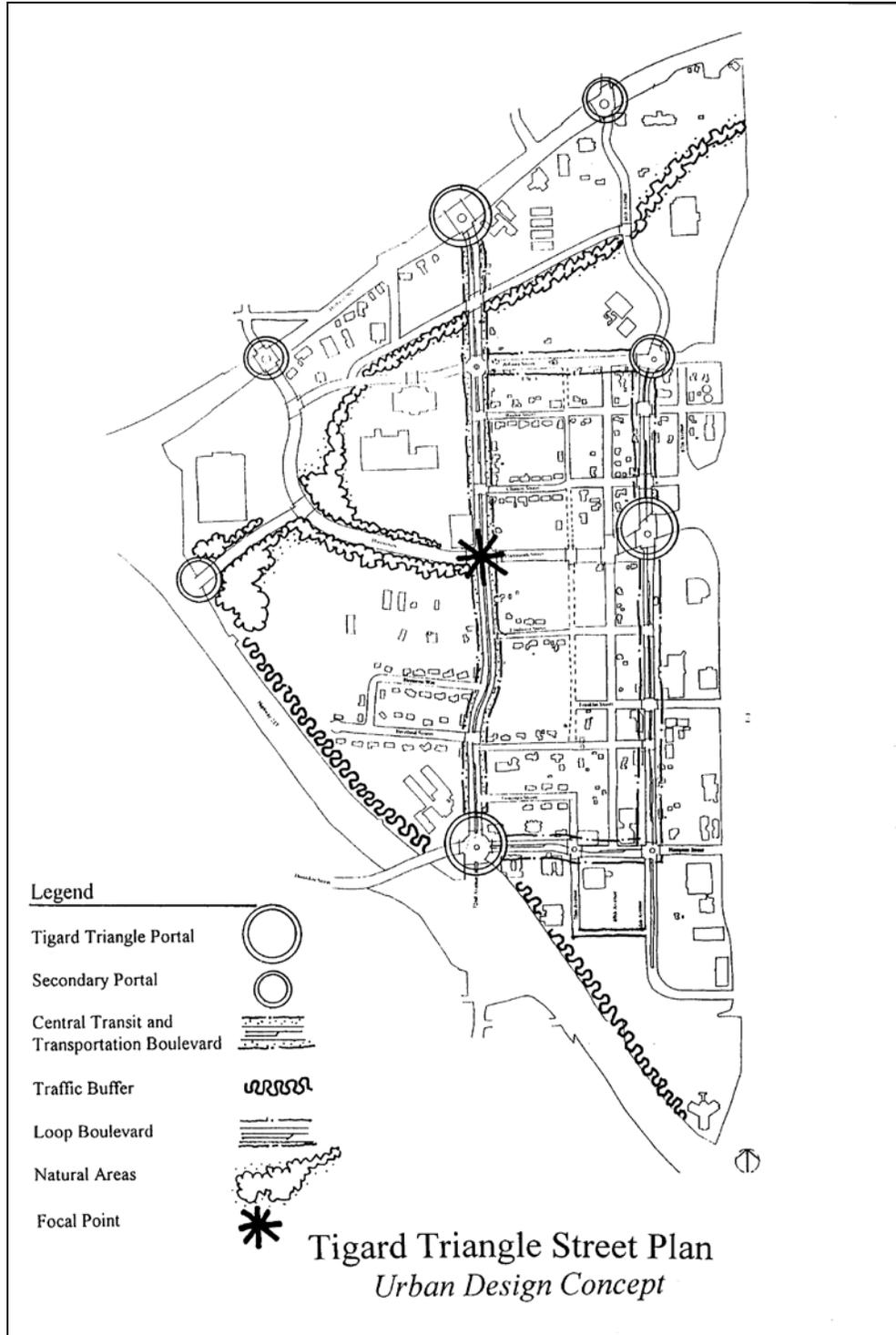
Function:	Pedestrian
Right-of-way	10' - 25' case-by-case
Ownership:	Private

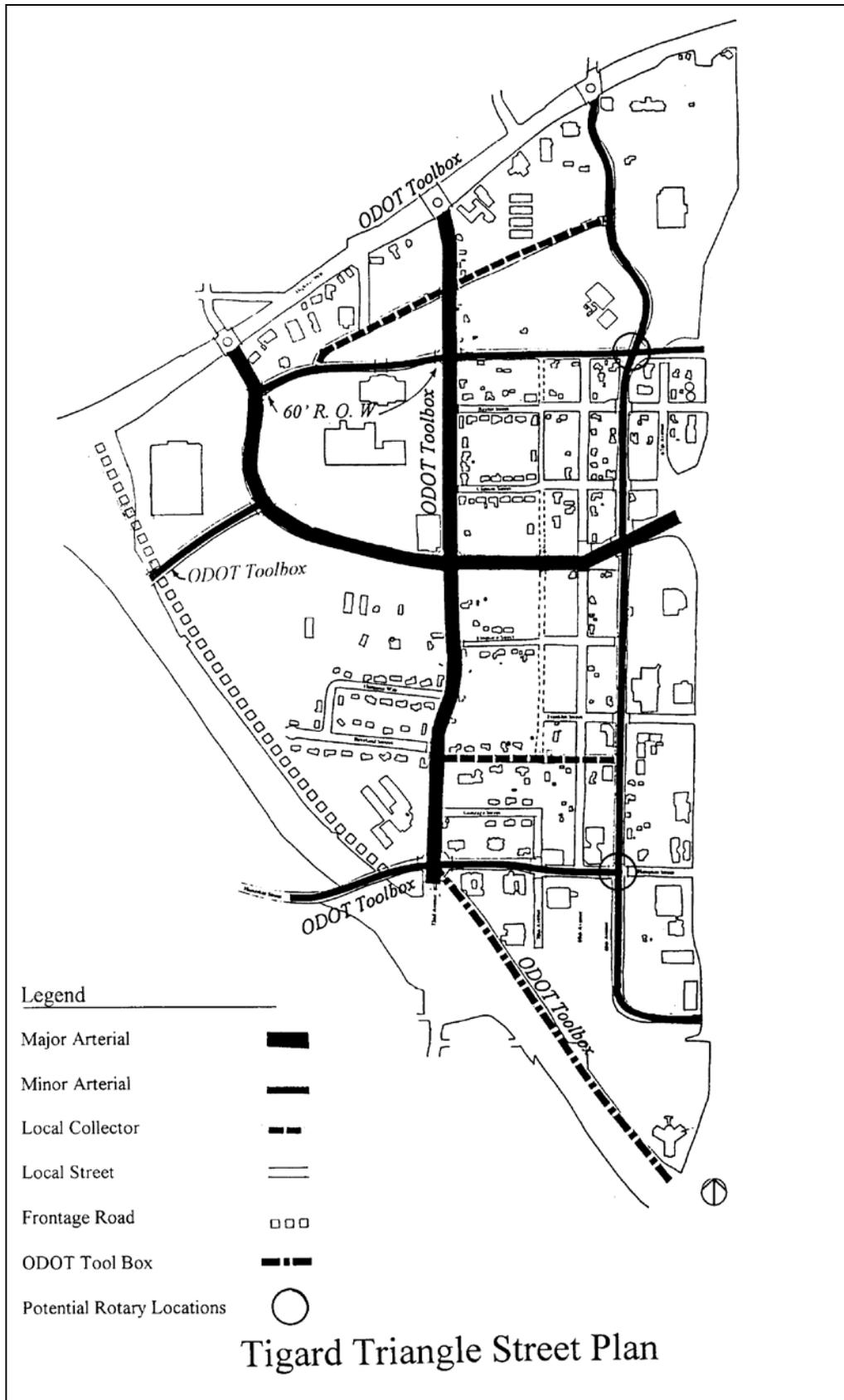
18.620.090 Design Evaluation

- A. Purpose. It is recognized that the above design standards are to assist in upgrading and providing consistency to development within the Tigard Triangle Plan District. It is recognized that different designs may be used to meet the intent of the standards and purpose statement of the Tigard Triangle Plan District standards. With this in mind, applicants for development in the Tigard Triangle Plan District may choose to submit proposed projects which demonstrate compliance with the design standards or request adjustments from the plan district design standards and submit design plans for review and recommendation by a city design evaluation team. This option allows applicants to propose alternative designs to the plan district design standards that are consistent with the purpose of the standards. When a structure which has nonconforming elements is partially or totally damaged by fire or other causes beyond the control of the owner, the structure may be rebuilt using the same structure footprint without receiving an adjustment from design standards.
- B. Design evaluation team (DET). Evaluation of the adjustment to allow an alternative design is made by a three-person professional design team contracted by the city for professional design review. The DET shall consist of design professionals with experience in architecture, landscape architecture and civil engineering. This team is charged with balancing the purpose statements, goals and standards of the Tigard Triangle Plan District design process with the alternative proposal submitted by the applicants. The DET shall accept design proposals that vary from any of the plan district design standards. This process is to be applied only to the Tigard Triangle Plan District design standards. Applicants must comply with all other development code standards according to the regular development review requirements of Title 18 of this code. The DET will prepare a report outlining conditions and recommendations in response to the applicant's proposal(s) for submission to the planning commission within 30 days of meeting on the proposal.
- C. Approval criteria. For guidance in evaluating the purpose of the design standards, the DET shall refer to the planning director's interpretation that provides purpose statements for the Tigard Triangle Plan District design standards. All adjustments to allow an alternative design are subject to the following criteria:

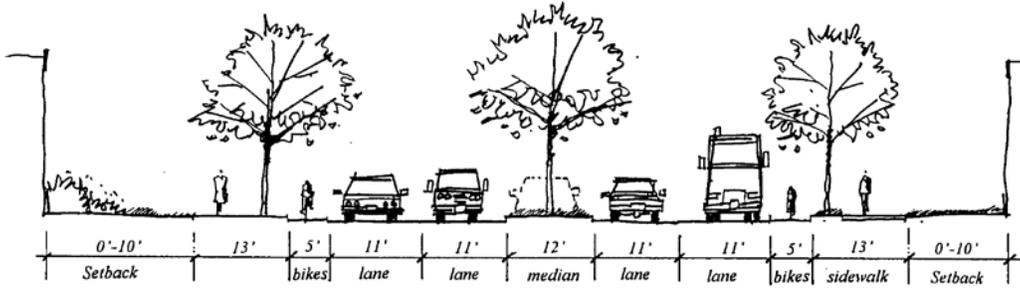
1. Granting the adjustment will continue to meet the purpose of the standard(s) to be modified in an acceptable alternative manner; and
 2. The proposal will not significantly detract from the livability or appearance of an area and the proposal will be consistent with the desired character of the area; and
 3. If more than one adjustment is being requested, the cumulative effect of the adjustments as well as each individual adjustment results in a project which is still consistent with the overall purpose, goals and standards of the zone; and
 4. Granting the adjustment is the minimum necessary to allow the proposed use of the site, and any impacts resulting from the adjustment are mitigated to the extent practical.
- D. Review process. The following steps must be followed by applicants to gain design evaluation review:
1. Applicants choosing the design evaluation process must submit a design evaluation and adjustment request according to a list of requirements provided by the director.
 2. Members of the design evaluation team are available to meet with applicants as part of the pre-application process; however, applicants shall pay for the entire cost of the review of the design evaluation team. A deposit of \$1,000 shall be paid upon application. The applicant will be billed for any additional cost. The DET report shall not be issued until all costs are paid. No request for design evaluation review using the alternative design will be accepted until all costs are paid in full.
 3. The applicant will receive a review date for a DET work session which shall be within 30 days of paragraph 2 of this subsection D. No public notification is required although the review session is open to the public. The review is designed to allow the applicant to present and explain design intent and adjustment proposals to the design evaluation team. This is not intended to be a public hearing and no public testimony will be taken.
 4. Upon completion of the DET review and payment of all costs, the DET will forward a report and recommendations to the director and the applicant within 30 days of meeting on the proposal. At the request of the applicant, this time period may be extended.
 5. The applicant may proceed to schedule and hold a pre-application neighborhood meeting with the adjacent property owners at any time during this process according to the provisions provided by the director at the development review pre-application conference required by Chapter 18.390. It is recommended that the development design to be reviewed at the pre-application neighborhood meeting include the recommendations of the DET.
 6. Upon completion of the neighborhood meeting requirements and receipt of the DET report, applicants may proceed to file the appropriate development application according to the provisions of Title 18 of this code. Said application shall include the recommendations of the DET.
 7. Review of the DET recommended plan and/or conditions shall be made part of the staff report prepared by the planning director and shall be made available at a public hearing before the planning commission according to the provisions of Chapter 18.390.

8. The planning commission may approve, approve with conditions or deny the development application considering the DET recommendation and evaluating the development and the design plan to ensure consistency with the Tigard Triangle Plan District design standards. Approval of the planning commission must also be based on compliance of the development plan with all other development code requirements governing the application.



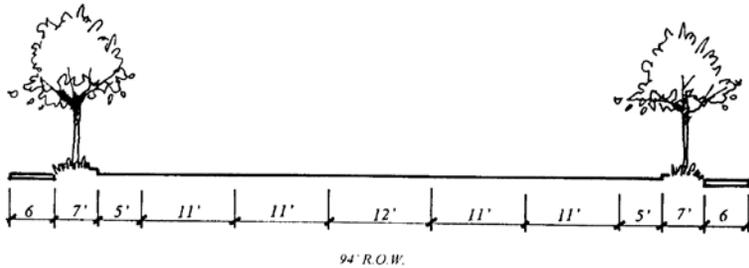
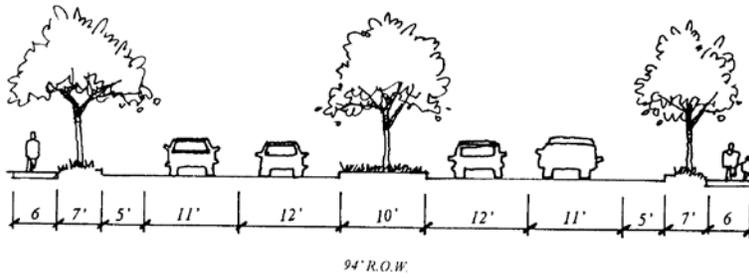


Street Widths: 92 Foot Right-of-Way, with Setback - 72nd Avenue

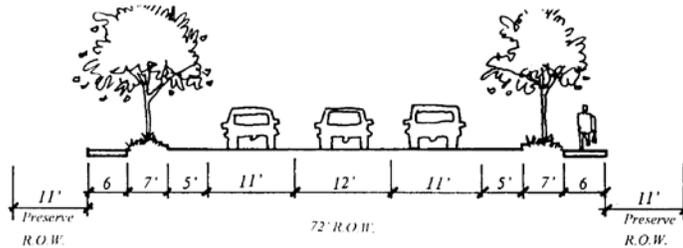


*NOTE: Provide U-turn capability for streets with medians. Locations and configurations to be determined during detail-design.

Street Widths: 94' Right of Way, Dartmouth Road, west of 72nd Ave.

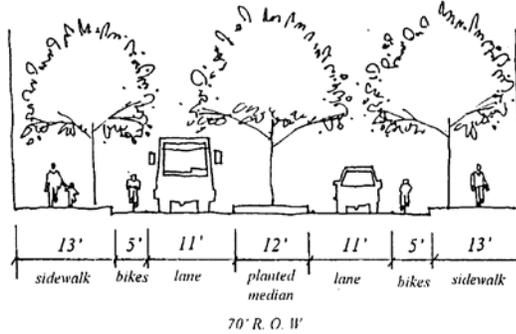


Street Widths: 72' Right of Way, Dartmouth Road, east of 72nd Ave.



Tigard Triangle Street Plan
Street Sections

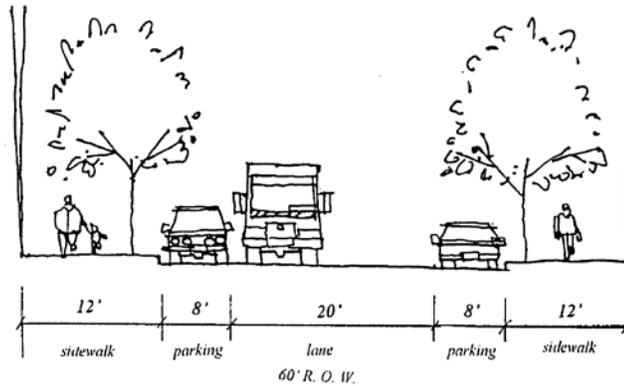
Street Widths: 70 Foot Right-of-Way - 68th Avenue, Hampton and Atlanta*



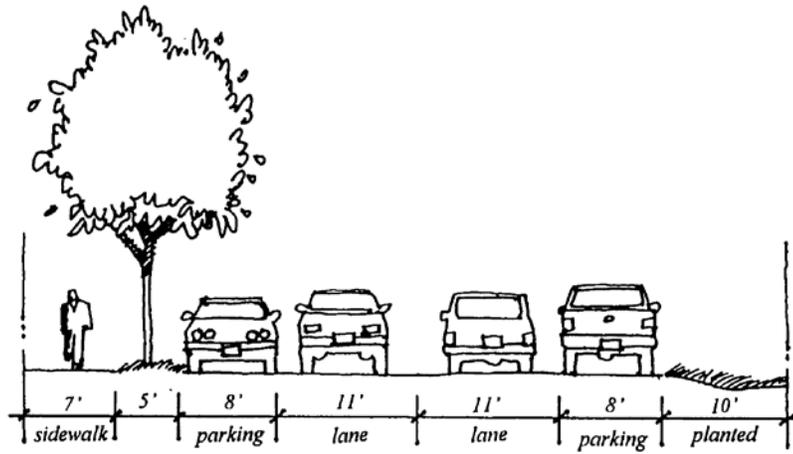
*NOTE: 68th Ave. from Highway 99 to Hampton
 Hampton from 68th Ave. to 72nd Ave.
 Atlanta from 68th Ave. to 72nd Ave.

Provide U-turn capability for streets with medians. Locations and configurations to be determined during detail-design.

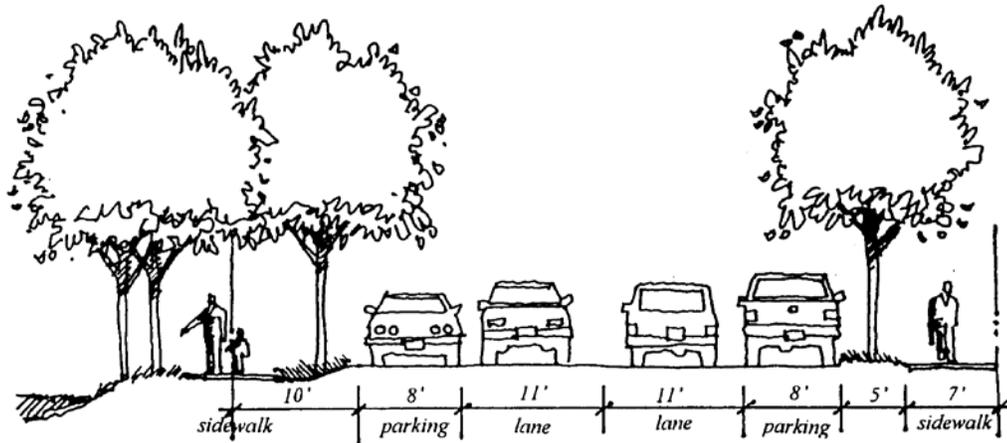
Typical 60 Foot Right-of-Way - Local Street



Tigard Triangle Street Plan
 Street Sections



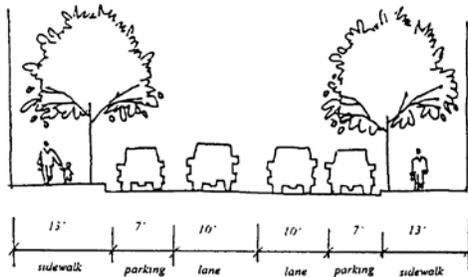
66th Avenue



Backage Road

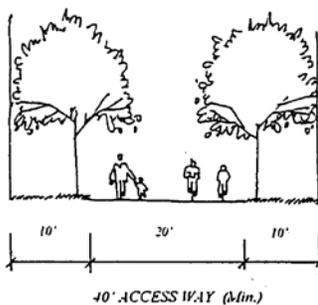
Tigard Triangle Street Plan
Street Sections

Access Way: Sections

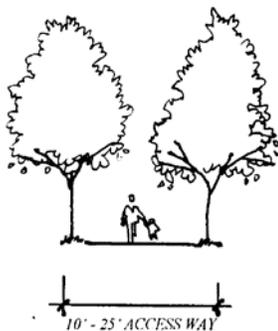


52' - 60' Access Way - 28' curb-to-curb Right-of-Way minimum*

*Public right-of-way includes parking and travel lanes. Single side parking optional.



40' ACCESS WAY (Min.)



NOTE: 10' - 25' Class 3 and Class 4 Access Way sections are dimensionally the same.

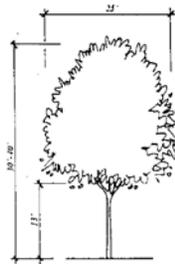
Tigard Triangle Street Plan
Street Sections

Landscape and Street Standards

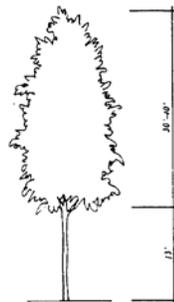
Landscape Standards: Street Trees

Street Name	Street Tree type	Min. Spacing	Location ¹
72 nd Avenue	Broad-spreading	27 feet o.c.	Between sidewalk and street
	Broad-spreading	27 feet o.c.	Center median
Dartmouth Street	Broad-spreading	27 feet o.c.	Between sidewalk and street
68 th Avenue	Columnar	22 feet o.c.	Between sidewalk and street
	Broad-spreading	22 feet o.c.	Center median
Atlanta Street	Columnar	22 feet o.c.	Between sidewalk and street
	Broad-spreading	22 feet o.c.	Center median
Hampton Street	Columnar	22 feet o.c.	Between sidewalk and street
	Broad-spreading	22 feet o.c.	Center median
66 th Avenue	Broad-spreading	27 feet o.c.	Between sidewalk and street on the west side of the street.
Backage Road	Broad-spreading	27 feet o.c.	Between sidewalk and street. Plant trees random and in clumps along the south side of the street. Provide a variety of species.
Local Streets	Spreading to 25 feet	22 feet o.c.	Between sidewalk and street
Portals	Columnar	22 feet o.c.	Plant trees to frame portal features and architecture.
Parking lots	Broad-spreading	1 per 7 spaces	In planter islands
	Broad-spreading	27 feet o.c.	In setbacks
	Columnar	22 feet o.c.	In setbacks where building lines preclude broad-spreading trees.

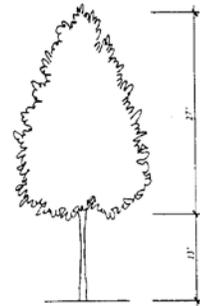
¹ All curb-side planting strips and medians shall be planted with lawn or groundcover. Planter strips between the sidewalk and street along 72nd Avenue shall be lawn except where paved areas extend to the curbline and tree grates are provided around trees. Medians on 68th Avenue, Atlanta and Hampton shall have consistent lawn or groundcover plantings for the entire length of the street with accents at intersections.



Broad-spreading Street Tree



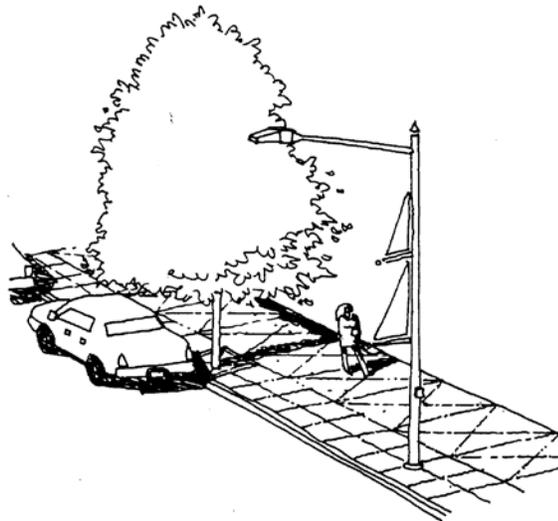
Columnar Street Tree



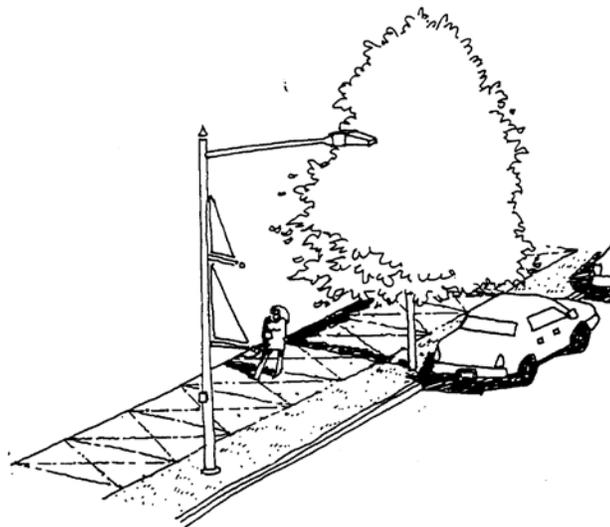
Spreading Street Tree

Tigard Triangle Street Plan Details

Landscape and Street Standards



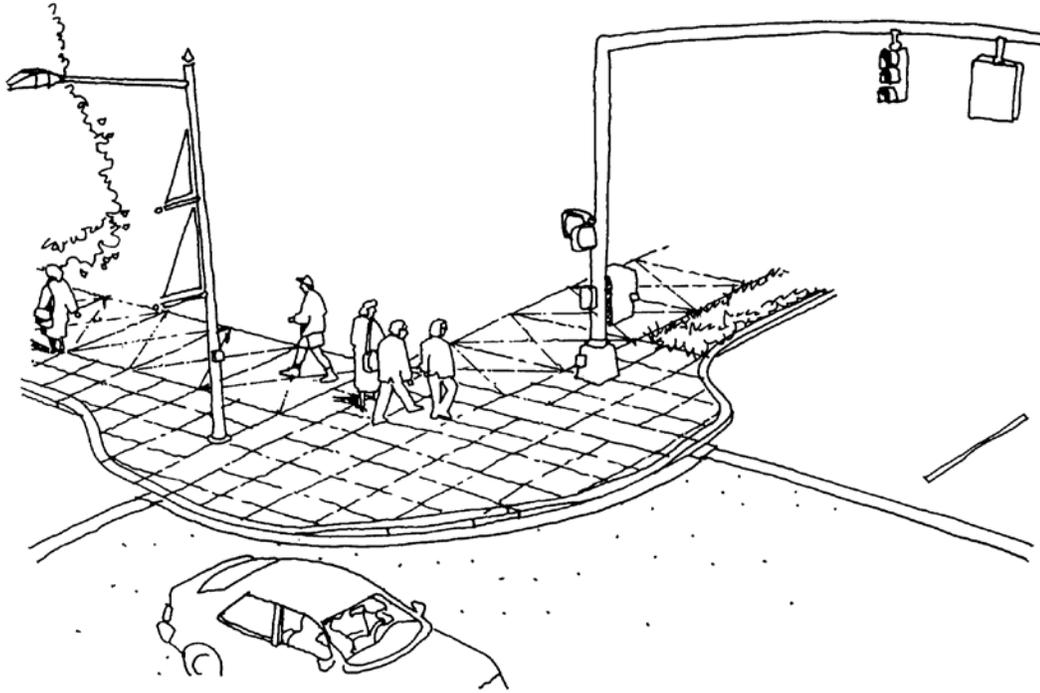
Paving to the curb with street tree grates or groundcover planting, street trees and street lights.



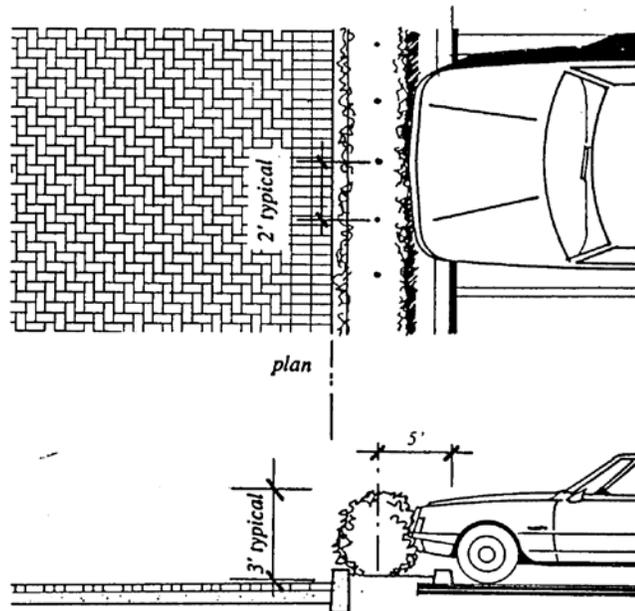
4 Foot Planter Strip

Tigard Triangle Street Plan
Details

Landscape and Street Standards



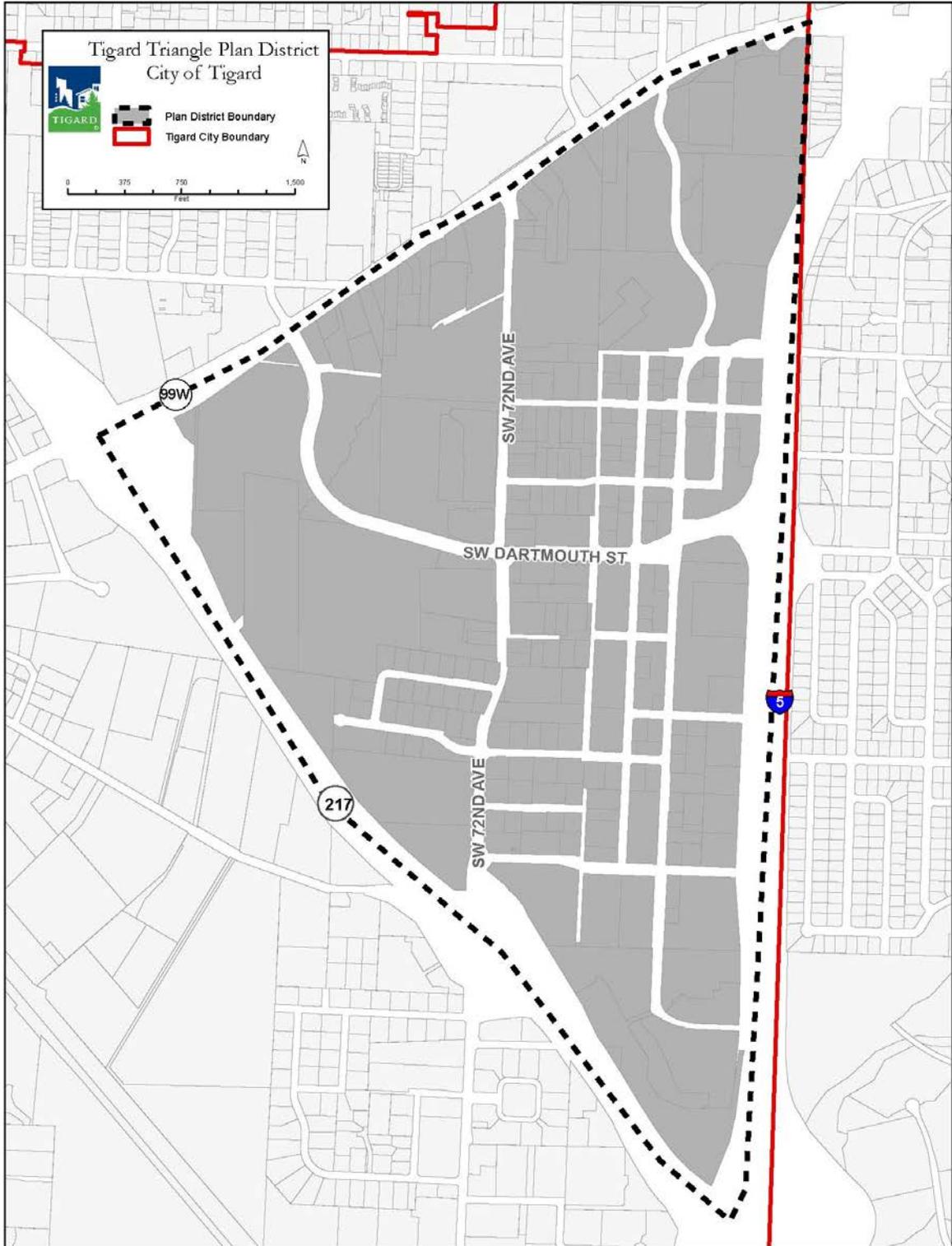
Curb extension and concrete intersection at intersections on Major and Minor Arterials.



Typical landscape and parking at minimum setback.

Tigard Triangle Street Plan
Details

Map 18.620.A: Tigard Triangle Plan District Boundaries



(Ord. 13-04 §1) ■